

SITE AND SURVEY CONTROL PLAN
 SCALE: 1" = 40'

LEGEND

- SURVEY CONTROL POINT
- CORNER MONUMENT
- EXISTING PROPERTY LINE/RIGHT-OF-WAY

SURVEY NOTES

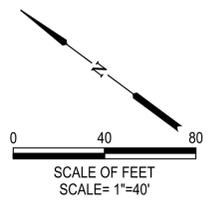
- HORIZONTAL AND VERTICAL CONTROL SURVEY REFERENCE POINTS ARE PROVIDED FOR CONTROL OF THE PROJECT BASED ON TOPOGRAPHIC SURVEY COMPLETED BY GIBBS AND OLSON ON MARCH 1, 2022 AND SUPPLEMENTED ON FEBRUARY 1, 2023. ALL LAYOUT AND CONSTRUCTION SURVEYING SHALL BE CONDUCTED BY A COMPANY UNDER THE SUPERVISION OF A PROFESSIONAL LAND SURVEYOR REGISTERED IN WASHINGTON.
- PRIOR TO BEGINNING ANY LAYOUT, THE CONTRACTOR'S SURVEYOR SHALL OCCUPY ALL REFERENCE CONTROL POINTS SHOWN ON THIS SHEET AND VERIFY DATA GIVEN. ANY DISCREPANCY SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEER'S ATTENTION FOR CLARIFICATION OR CORRECTION. NOTES SHOWING CONFIRMATION OF THE HORIZONTAL AND VERTICAL IN ACCORDANCE WITH THE GENERAL CONTRACT PROVISIONS SHALL BE PROVIDED TO THE ENGINEER PRIOR TO STARTING CONSTRUCTION.
- WORK PERFORMED USING A 2 SECOND TOPCON GT-502 ROBOTIC TOTAL STATION, TOPCON HIPER-VR GPS-RTK EQUIPMENT, PRECISION EXCEEDS REQUIREMENTS OF W.A.C. 332-130-090
- THE PURPOSE OF THIS TOPOGRAPHIC SURVEY IS FOR CIVIL ENGINEERING AND ARCHITECTURAL DESIGN. THIS IS NOT A BOUNDARY SURVEY.
- MT. VIEW LOCATING SERVICE, LLC, (PO BOX 40, SUMNER, WA 98390), WAS CONTRACTED TO MARK UTILITIES IN SELECT AREAS AS DIRECTED BY GIBBS AND OLSON, (1157 3RD AVENUE, LONGVIEW, WA 98632). WASHINGTON UTILITY NOTIFICATION CENTER MUST BE NOTIFIED AT 800-424-5555 OR 811 PRIOR TO ANY CONSTRUCTION OR UNDERGROUND UTILITY LOCATION. UTILITY LOCATIONS ARE GATHERED BY MEASUREMENTS TO SURFACE MARKS AND LOCATION PAINT PROVIDED BY MT. VIEW LOCATING SERVICES, LLC IN THE FIELD. SURVEYOR MAKES NO GUARANTEE OF THE UNDERGROUND UTILITIES SHOWN IN THE AREA. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED.
- EXISTING SITE FEATURE PORTRAYED AS BACKGROUND INFORMATION ON ALL PLANS IN THIS DRAWING SET ARE BASED ON THE BEST INFORMATION AVAILABLE AND WERE COMPILED USING INFORMATION OBTAINED FROM RECORD DRAWINGS PROVIDED BY THE OWNER, GIS, AND A TOPOGRAPHIC SURVEY. CONTRACTOR SHALL VERIFY ALL EXISTING SITE CONDITIONS IN THE FIELD AND NOTIFY THE ENGINEER IMMEDIATELY IF THERE ARE DISCREPANCIES TO WHAT IS SHOWN ON THESE PLANS.

BASIS OF BEARING:

N36°56'38"W BETWEEN MONUMENTS NO. 1 AND NO. 3, WASHINGTON PLANE COORDINATE SYSTEM (WPCS), SOUTH ZONE 4602, NAD 83/2011 (EPOCH: 2010), GEIOD: G2012BU1, NAVD88, U.S. FEET, DERIVED FROM WASHINGTON STATE REFERENCE NETWORK (WSRN) STATION: CROK3

MONUMENT NO. 1 N: 295740.20 E: 1032362.26 ELEV: 14.60 LAT: N046° 07' 08.3175" LON: W122° 53' 53.5079"	MONUMENT NO. 3 N: 296267.62 E: 1031965.63 ELEV: 14.86 LAT: N046° 07' 13.4028" LON: W122° 53' 59.3638"
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UNLESS OTHERWISE NOTED, DISTANCES ARE GROUND DISTANCES.
 TO CALCULATE GRID DISTANCE, MULTIPLY GROUND DISTANCE BY THE COMBINED SCALE FACTOR: 0.99994679



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VERIFY SCALES
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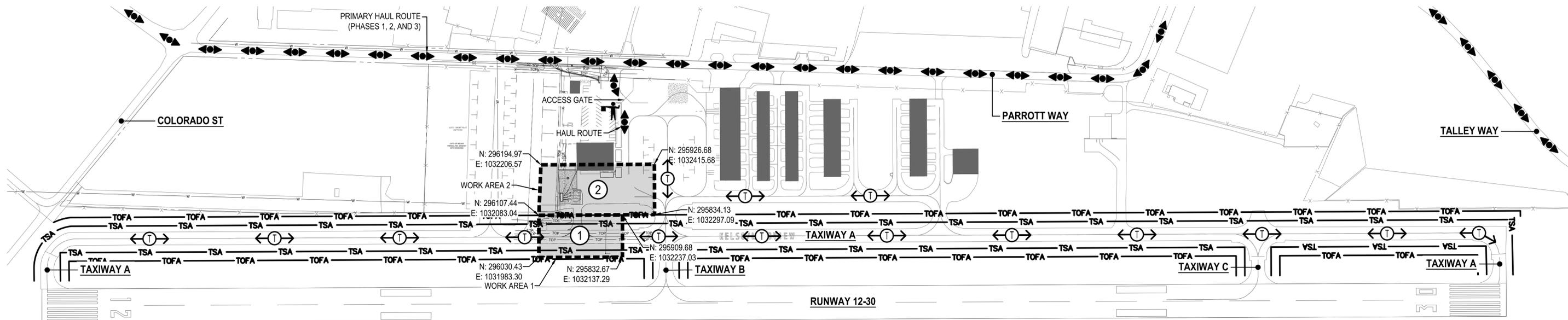
DATE: APRIL 2024
 PROJECT NO: 35005.004.03

DESIGNED BY: TJS
 DRAWN BY: TJS
 CHECKED BY: CRL
 SCALE: AS NOTED

**SOUTHWEST WASHINGTON REGIONAL AIRPORT
 TASK ORDER #4 - FINAL DESIGN
 APRON/TAXILANE RECONSTRUCTION**

SITE AND SURVEY CONTROL PLAN

DRAWING NO. **G-02**
 SHEET NO. **2 OF 15**



LEGEND

- CONTRACTOR ACCESS AND HAUL ROUTE
- AOA SECURITY GUARD
- LOW PROFILE BARRICADE
- CANDLESTICK BARRICADE
- TSA TAXIWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA

GENERAL NOTES

1. ALL WORK SHALL BE SEQUENTIAL AS NOTED IN THE "CONSTRUCTION PHASING & OPERATION CONSTRAINTS" TABLE. CONCURRENT WORK SHALL NOT BE ALLOWED.
2. AREAS WHERE ACCESS/HAUL ROUTES CROSS ACTIVE AIRCRAFT AREAS AND IN FRONT OF EXISTING HANGARS SHALL BE CONTINUOUSLY MONITORED FOR FOD. THE CONTRACTOR SHALL NOT, UNDER ANY CIRCUMSTANCE, BLOCK ACCESS TO THESE HANGARS.
3. CONTRACTOR'S ACCESS ROAD, HAUL ROADS, AND STAGING AREAS SHALL BE CONSTRUCTED AS NEEDED, AT CONTRACTOR'S EXPENSE. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION AT NO ADDITIONAL COST TO THE OWNER.
4. ALL PAVEMENT SURFACES SHALL BE PROTECTED FROM DAMAGE DUE TO CONSTRUCTION ACTIVITY. RESTORE DAMAGED PAVEMENT TO ORIGINAL OR BETTER CONDITION AT NO COST TO THE OWNER.
5. CONTRACTOR SHALL CONTROL DUST AND SMOKE RELATED TO CONSTRUCTION ACTIVITIES AT ALL TIMES.
6. ACTIVE AIRCRAFT SURFACES SHALL BE KEPT CLEAN AS WORK PROGRESSES AND SHALL BE FREE OF FOD (FOREIGN OBJECT DEBRIS), BEFORE THESE SURFACES ARE OPENED TO AIRCRAFT OPERATIONS.
7. WHENEVER DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL BEGIN CLEANUP OPERATIONS OR SHUT DOWN CLEANUP OPERATIONS ALREADY IN PROGRESS. IN THE EVENT THE AIRPORT OR ENGINEER DETERMINES ANY ACTIVE AIRCRAFT MOVEMENT AREA TO BE UNUSABLE OR HAZARDOUS DUE TO CONSTRUCTION GENERATED DEBRIS, AND THE CONTRACTOR IS NOT ON SITE OR NOT ABLE TO RESPOND PROMPTLY, CLEANUP EQUIPMENT WILL BE CALLED TO THE AIRPORT. THE RESULTING COST WILL BE RECOVERED AS A LIQUIDATED DAMAGE AGAINST THE CONTRACTOR.



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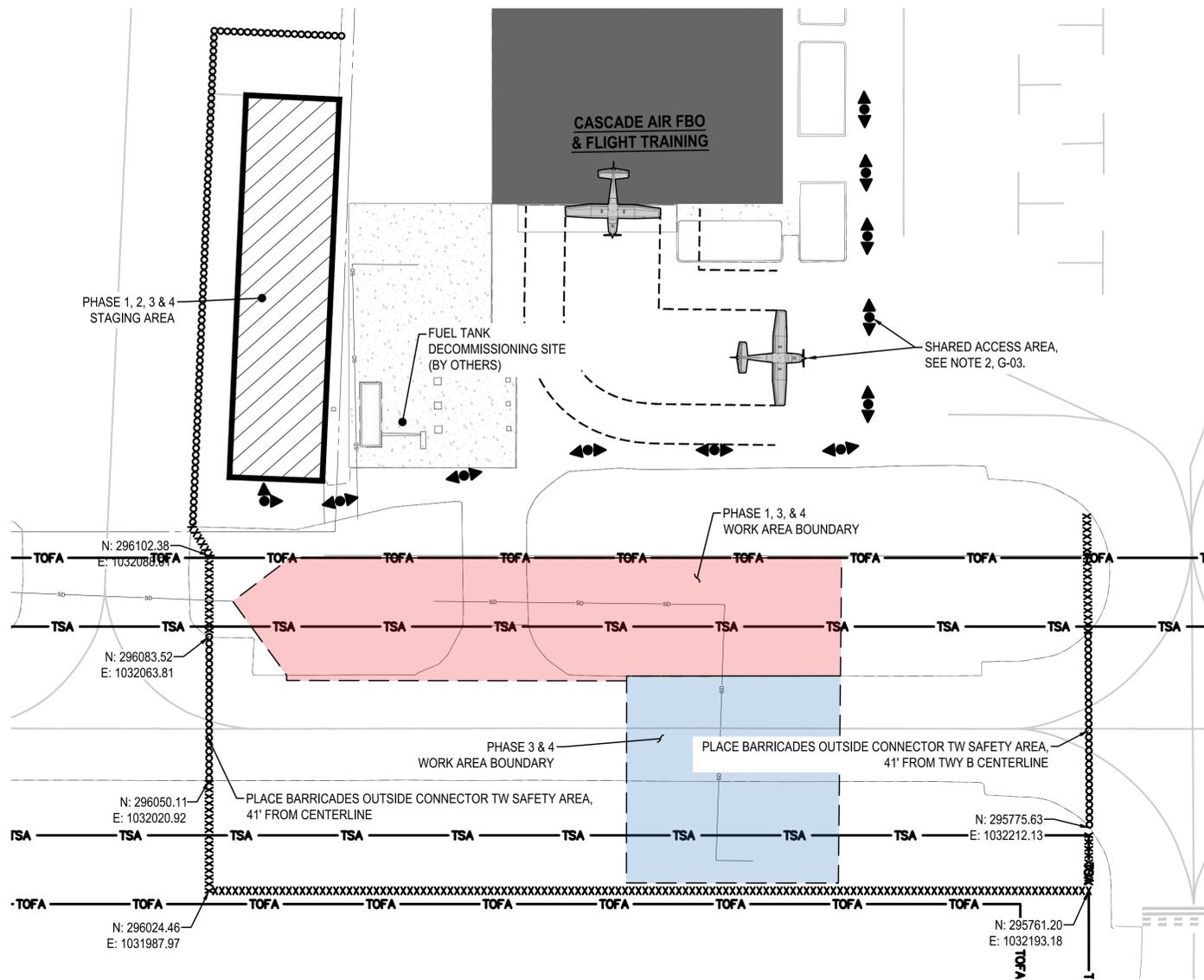
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**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION**

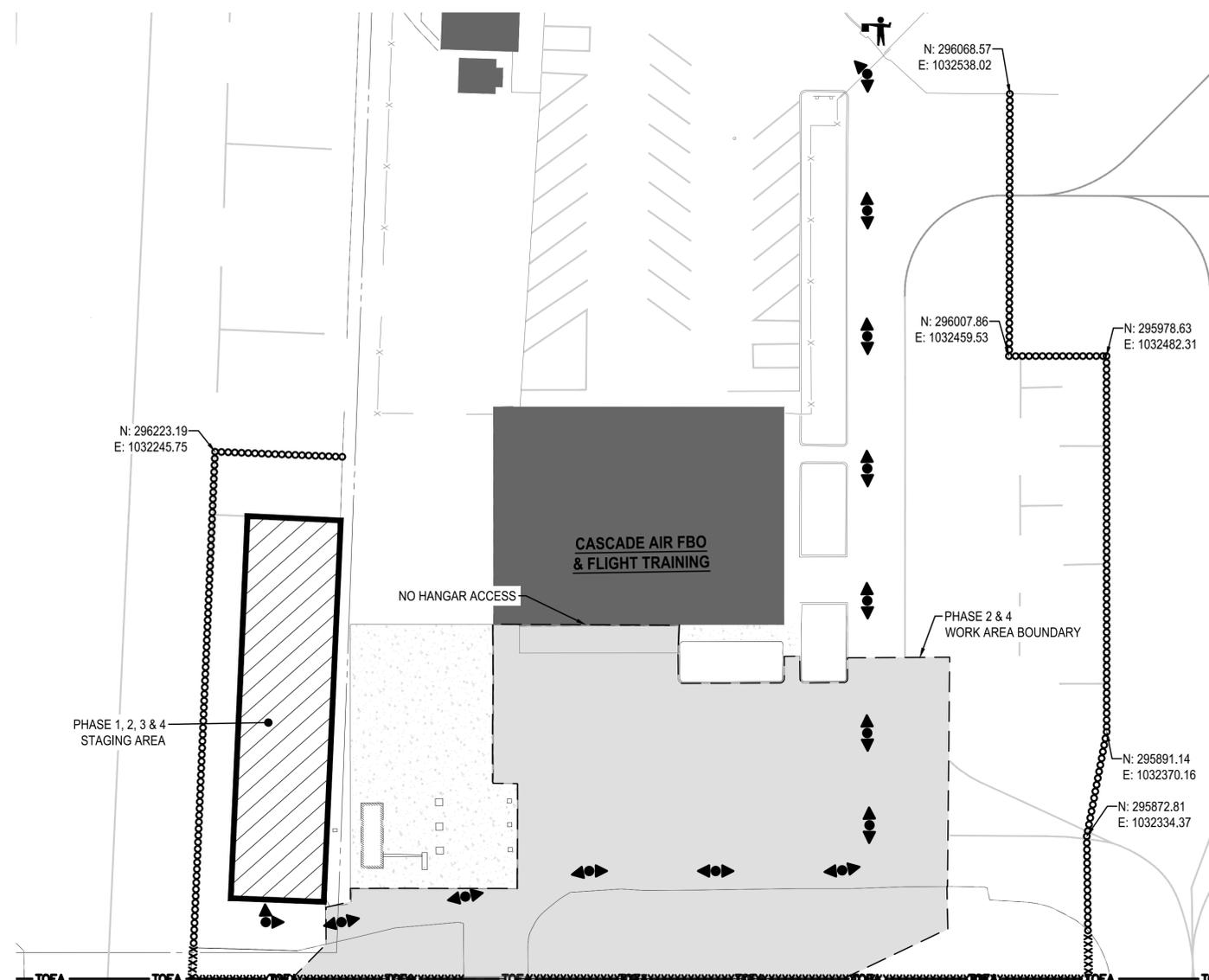
SAFETY AND PHASING PLAN OVERVIEW

DRAWING NO. **G-03**
SHEET NO. **3 OF 15**



WORK AREA 1 PLAN

SCALE: 1" = 30'



WORK AREA 2 PLAN

SCALE: 1" = 30'



CONSTRUCTION PHASING & OPERATION CONSTRAINTS									
PROJECT PHASE	DURATION	WORK AREA	SEQUENCE	WORK SCOPE	RUNWAY STATUS	TAXIWAY STATUS			FBO HANGAR ACCESS
						RW 12-30	TW A	TW B	
1	14	1	1	- ASPHALT REMOVAL - GRADING - STORM/SUBDRAIN - BASE ROCK	OPEN	PARTIAL CLOSURE	OPEN	OPEN	OPEN
2	18	2	2	- ASPHALT REMOVAL - GRADING - STORM/SUBDRAIN - BASE ROCK	OPEN	OPEN	OPEN	OPEN	CLOSED
3	5	2	3	- ASPHALT REMOVAL - GRADING - STORM - BASE ROCK	OPEN	PARTIAL CLOSURE	OPEN	OPEN	CLOSED
4A	3	1 & 2	4	- PAVING - MARKING - HYDROSEEDING	OPEN	PARTIAL CLOSURE	OPEN	OPEN	CLOSED
4B	1	1 & 2	5	- FINAL MARKING	OPEN	PARTIAL CLOSURE	OPEN	OPEN	CLOSED

LEGEND

- PROPOSED CONTRACTOR STAGING AREA BY PHASE
- CONTRACTOR ACCESS AND HAUL ROUTE
- AOA SECURITY GUARD
- WORK AREA BOUNDARY

LEGEND CONT.

- LOW PROFILE BARRICADE
- CANDLESTICK BARRICADE
- TSA TAXIWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA



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SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

SAFETY AND PHASING PLAN - WORK AREAS 1 & 2

DRAWING NO.
G-04

SHEET NO.
4 OF 15

GENERAL WORK AREA NOTES:

1. THE CONTRACTOR SHALL BE RESTRICTED TO USE THE ENTRANCE AND HAULING ROUTES SHOWN ON THE DRAWINGS. FOLLOW AIRPORT AND FAA SAFETY PROCEDURES WHEN MOVING EQUIPMENT OR PERSONNEL. NO PERSONAL VEHICLES SHALL BE ALLOWED OUTSIDE OF THE STAGING AREA. THE AIRPORT MAY IMMEDIATELY REMOVE ANY PERSONNEL AND EQUIPMENT FROM THE SITE IN VIOLATION OF AIRPORT SAFETY AND SECURITY PROCEDURES.
2. COVER GUIDANCE SIGNS AS DIRECTED BY THE AIRPORT TO PREVENT AIRCRAFT FROM TAXIING TOWARDS CLOSED WORK AREAS.
3. LIMIT EQUIPMENT HEIGHT TO 30 FEET UNLESS OTHERWISE APPROVED BY THE ENGINEER.
4. LIMIT STOCK PILES TO 10 FEET IN HEIGHT AND AT LEAST 250 FEET FROM RUNWAY CENTERLINE AND 66 FEET FROM TAXIWAY CENTERLINE.
5. IN THE EVENT OF AN EMERGENCY, MOVE ALL EQUIPMENT AND PERSONNEL TO THE CONTRACTOR'S STAGING AREA UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
6. ACCESS TO ANY WORK AREA MUST BE AUTHORIZED BY THE ENGINEER PRIOR TO WORK IN THAT AREA. NOTIFY THE ENGINEER A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK IN ANY AREA. NOTAMS SHALL BE ISSUED BY THE AIRPORT.
7. PLACE LOW LEVEL BARRICADES AS SHOWN AND DESCRIBED IN SECTION 01300 OF THE SPECIFICATIONS.
8. BARRICADES SHALL BE PLACED PRIOR TO BEGINNING CONSTRUCTION OPERATIONS AND SHALL BE REMOVED WHEN TAXILANES ARE REOPENED.
9. RETURN ALL EQUIPMENT TO STAGING AREA AT THE END OF EACH WORK DAY.
10. FOR ADDITIONAL REQUIREMENTS, REFER TO SECTION 01330, AIRPORT SAFETY, IN THE TECHNICAL SPECIFICATIONS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE TO LOCATE AND PROTECT ALL UTILITIES DURING THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING THE UTILITY NOTIFICATION CENTER TO LOCATE PUBLIC UTILITIES AND FOR ANY ADDITIONAL UTILITY LOCATIONS INCLUDING HIRING A PRIVATE LOCATE SERVICE IF REQUIRED. ANY UTILITIES DAMAGED IN CONJUNCTION WITH THE CONSTRUCTION ACTIVITIES SHALL BE REPLACED AND/OR REPAIRED BY THE CONTRACTOR AS APPROVED BY THE OWNER.
12. ALL HAUL ROUTES SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT. VERIFY CONDITION WITH OWNER PRIOR TO CONSTRUCTION.

WORK IN SAFETY AREAS AND OBJECT FREE AREAS:

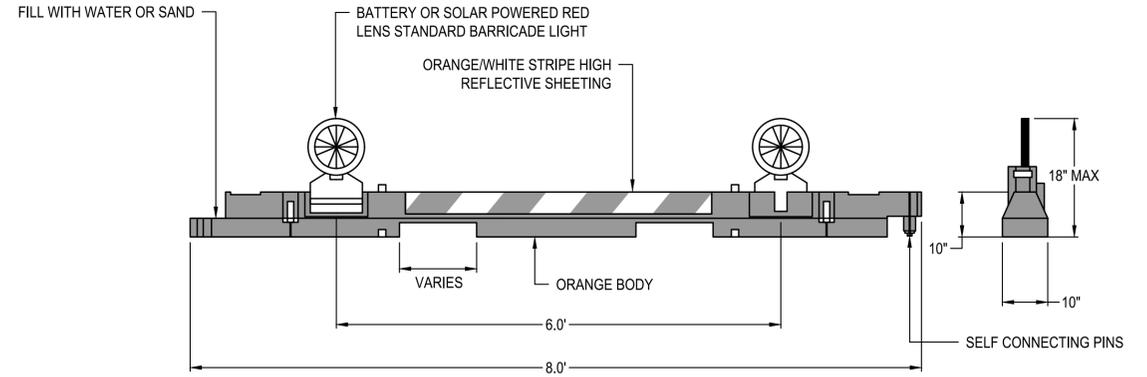
1. NO WORK IS ALLOWED IN ACTIVE RUNWAY AND TAXIWAY SAFETY AREAS. RUNWAY SAFETY AREA (RSA) AND TAXIWAY SAFETY AREA (TSA) ARE DEFINED AS AREAS THAT SHALL BE CLEARED AND GRADED AND HAVE NO RUTS, HUMPS, OPEN TRENCHES, DEPRESSIONS, OR OTHER SURFACE VARIATIONS. THE MAXIMUM SLOPE ANYWHERE WITHIN A RUNWAY OR TAXIWAY SAFETY AREA SHALL BE 3%. IN TRANSITIONS FROM PAVED TO UNPAVED AREAS, A 1.5 INCH VERTICAL DROP IS ALLOWED. THE RUNWAY AND TAXIWAY SAFETY AREAS SHALL BE MAINTAINED AT ALL TIMES WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC. PERSONNEL, EQUIPMENT, OR MATERIAL WITHIN A RUNWAY SAFETY AREA AT ANY TIME REQUIRES A CLOSURE. SAFETY AREAS THAT ARE CLOSED FOR CONSTRUCTION MUST MEET THESE CRITERIA PRIOR TO REOPENING.
2. THE CONTRACTOR SHALL ANTICIPATE THE CONSTRUCTION OF TEMPORARY FILLS, COMPACTION, TRENCH BACKFILLING, AND GRADING TO MEET THE REQUIREMENTS OF "WORK IN SAFETY AREAS AND OBJECT FREE AREAS", PRIOR TO REOPENING RUNWAYS AND TAXIWAYS. THIS WORK IS CONSIDERED INCIDENTAL TO VARIOUS WORK ITEMS AND SEPARATE PAYMENT WILL NOT BE MADE.
3. CONTRACTOR SHALL NOT ENTER ANY ACTIVE RSA OR TSA WITHOUT AUTHORIZATION FROM THE AIRPORT. ALL EQUIPMENT, TOOLS, AND MATERIALS SHALL BE MOVED TO STAGING AREAS PRIOR TO REOPENING A RUNWAY OR TAXIWAY.
4. SAFETY AREA LIMITS
 TAXIWAY - 39.5' FROM FROM CENTERLINE
 RUNWAY 12-30 - 75' FROM CENTERLINE; 300' FROM THRESHOLD
5. RUNWAY AND TAXIWAY OBJECT FREE AREAS (OFA) ARE DEFINED AS AREAS THAT SHALL BE CLEAR OF FIXED OR MOVABLE OBJECTS. EQUIPMENT NOT IN USE AND MATERIAL STOCKPILES AND STORAGE SHALL BE PLACED AT LEAST 250 FEET FROM ANY RUNWAY CENTERLINE. NO WORK MAY OCCUR IN TAXIWAY OBJECT FREE AREAS UNLESS THE TAXIWAY IS CLOSED TO AIRCRAFT TRAFFIC. NO STORAGE OF EQUIPMENT, MATERIALS OR STOCKPILES IS ALLOWED WITHIN TAXIWAY OBJECT FREE AREAS.
6. OBJECT FREE AREA LIMITS
 RUNWAY 12-30 - 250' FROM CENTERLINE
 TAXIWAYS - 65.5' FROM CENTERLINE
7. RUNWAY AND TAXIWAY SAFETY AREA AND RUNWAY AND TAXIWAY OBJECT FREE AREA CRITERIA MUST BOTH BE MET PRIOR TO OPENING AN IMPACTED RUNWAY OR TAXIWAY.

SEQUENCING AND WORK SCHEDULE NOTES:

1. COORDINATE WORK AREA CLOSURES WITH THE AIRPORT TO ENSURE ACCESS AS NEEDED TO MAINTAIN AIRPORT OPERATIONS, INCLUDING PROVIDING FOR AND MAINTAINING ACCESS TO THE FOLLOWING:
 - ACCESS TO THE APRON AREAS
2. REFER TO SHEET G-04 FOR PHASE DURATIONS, WORK SCOPE AND SEQUENCING.
3. UNLESS NOTED OTHERWISE, WORK MAY BE 24/7 AT THE CONTRACTOR'S OPTION.

CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICE:

1. THE CONTRACTOR SHALL HAVE A FULL TIME SUPERINTENDENT ON THE PROJECT SITE ANY TIME WORK IS BEING ACCOMPLISHED ON THE AIRPORT. THIS INCLUDES WORK BEING PERFORMED BY SUBCONTRACTORS OR SERVICE PROVIDERS UNLESS OTHERWISE APPROVED BY THE OWNER.
2. THE CONTRACTOR SHALL ASSIGN ONE PERSON TO BE THE SAFETY OFFICER DURING THE PROJECT. THE SAFETY OFFICER SHALL BE ONSITE AT ALL TIMES WHEN WORK IS OCCURRING IN THE AOA WHEN THE AIRPORT IS OPEN UNLESS OTHERWISE APPROVED BY THE OWNER. THE SAFETY OFFICER SHALL BE RESPONSIBLE FOR THE EDUCATION AND TRAINING OF ALL PERSONNEL WHO WILL ACCESS AIRPORT PROPERTY. THE OFFICER SHALL MAINTAIN RECORDS INDICATING THAT PERSONNEL HAVE BEEN TRAINED AND ARE FAMILIAR WITH SAFETY RULES AND REGULATIONS RELATED TO OPERATIONS AT THE AIRPORT DURING CONSTRUCTION. PERSONNEL WHO VIOLATE SAFETY REQUIREMENTS MAY BE REMOVED FROM THE PROJECT AT THE DISCRETION OF THE OWNER. THE SAFETY OFFICER SHALL ALSO BE RESPONSIBLE FOR FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT.
3. THE CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER SHALL ATTEND THE WEEKLY CONSTRUCTION MEETING.

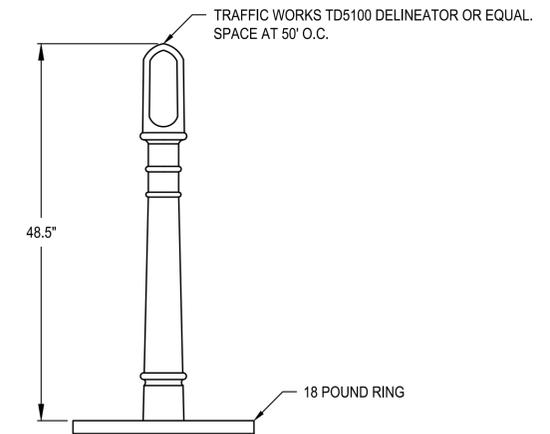


NOTES:

1. BARRICADES SHALL MEET THE REQUIREMENTS OF AC 150/5370-2, CURRENT EDITION.
2. PLACE BARRICADE PER THE PLANS OR AS DIRECTED BY THE RPR.
3. BARRICADES SHALL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING, FILLING, EMPTYING, MOVING, MAINTAINING AND PROTECTING BARRICADES THROUGHOUT THE DURATION OF THE PROJECT.
4. LIGHTS ARE TO BE NO FARTHER APART THAN 10' WHEN PLACED.
5. BARRICADES SHALL BE COVERED WITH REFLECTIVE SHEETING OR OTHER MATERIAL APPROVED BY THE RPR.
6. BARRICADES SHALL BE APPROPRIATELY WEIGHTED DOWN TO RESTRICT MOVEMENT FROM HIGH WINDS OR PROP WASH.

LOW LEVEL BARRICADE DETAIL

SCALE=NTS



BARRICADE DELINEATOR DETAIL (CANDLESTICK)

SCALE=NTS



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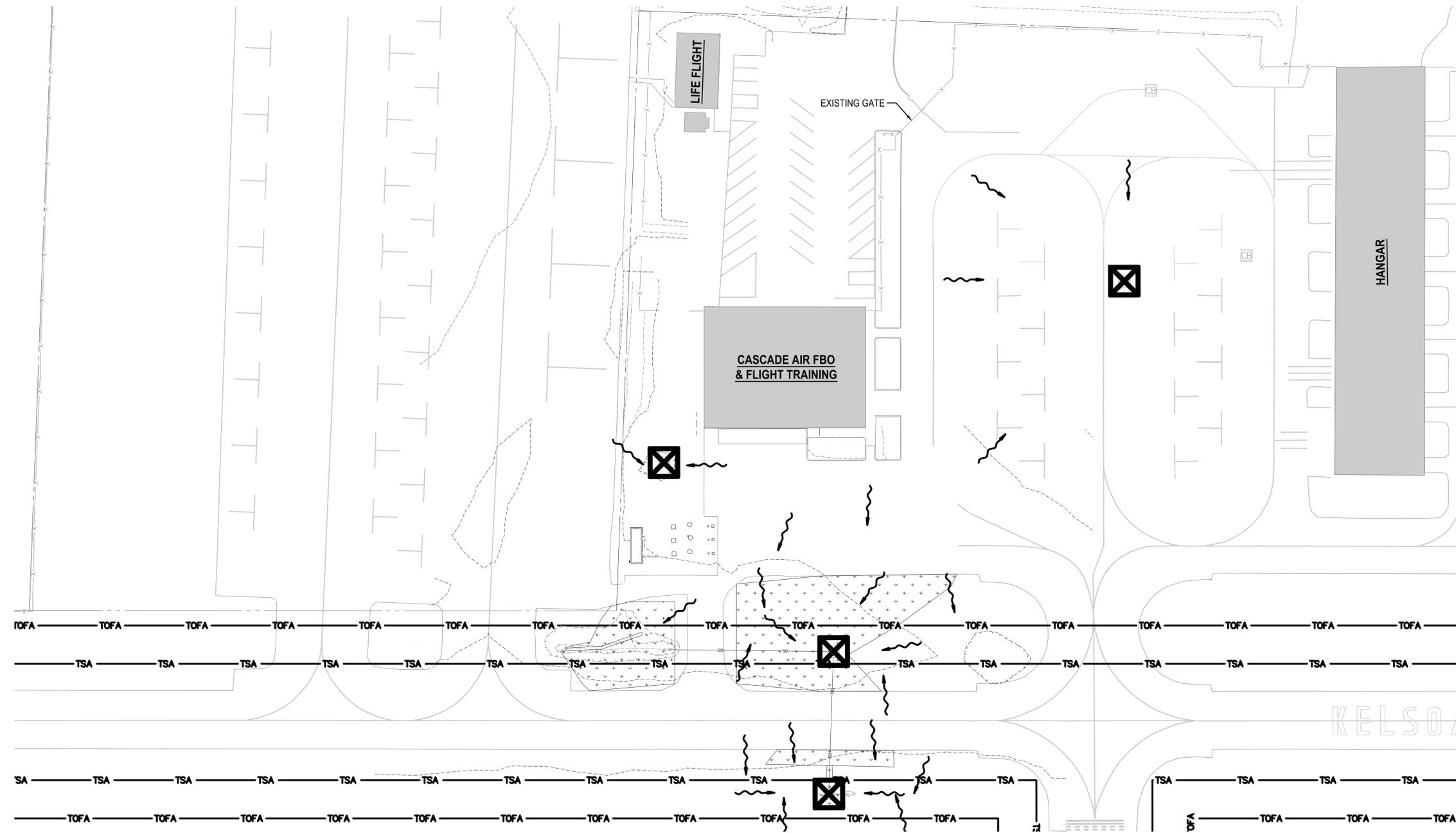
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**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION**

SAFETY AND PHASING NOTES

DRAWING NO.
G-05

SHEET NO.
5 OF 15



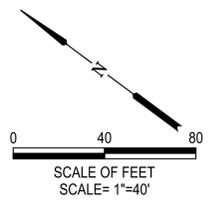
EROSION CONTROL PLAN 1
 SCALE: 1" = 40' C-01

LEGEND

- EXISTING EDGE OF PAVEMENT
- 15.0' EXISTING 1' CONTOUR
- EXISTING 0.2' CONTOUR
- TEMPORARY INLET PROTECTION
SEE WSDOT STANDARD PLAN I-40.20-00
- TOPSOIL & SEEDING LIMITS, SEE GENERAL NOTE 2.
- FLOW DIRECTION

GENERAL NOTES

1. TEMPORARY INLET PROTECTION SHALL BE REMOVED UPON COMPLETION OF CONSTRUCTION.
2. TOPSOIL & SEEDING SHALL BE PROVIDED FOR ALL AREAS BETWEEN THE GRADING LIMITS AND PROPOSED PAVEMENT REPLACEMENT AND ALL OTHER DISTURBED AREAS AS DIRECTED BY THE ENGINEER. REFER TO DETAIL 1 ON SHEET C-02.
3. DO NOT SEED WITHIN SWALE LIMITS (±2").



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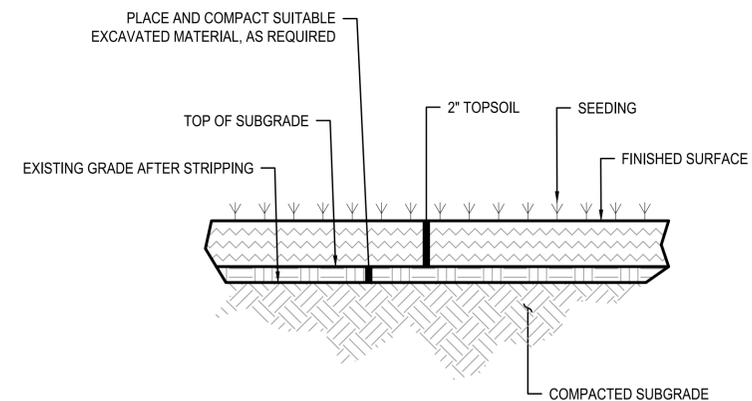
**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION**

EROSION CONTROL PLAN

DRAWING NO.
C-01

SHEET NO.
6 OF 15

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TYPICAL TOPSOIL AND SEEDING

SCALE=NTS



EROSION CONTROL GENERAL NOTES

1. ALL EROSION CONTROL DEVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THIS EROSION CONTROL PLAN AND THE EROSION CONTROL DETAILS, AND IN PLACE PRIOR TO THE START OF ANY LAND DISTURBING ACTIVITY.
2. ALL EROSION PREVENTION AND CONTROL BMP'S SHALL BE INSPECTED, MAINTAINED AND REPAIRED AS NEEDED THROUGHOUT CONSTRUCTION TO ENSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.
3. PERFORM ALL ACTIONS NECESSARY TO PREVENT EROSION AND CONTROL SEDIMENT, INCLUDING DUST, FROM LEAVING THE CONSTRUCTION SITE.
4. AS CONSTRUCTION PROGRESSES AND SEASONAL CONDITIONS DICTATE, MORE EROSION AND SEDIMENT CONTROLS MAY BE REQUIRED TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATERS DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR SURFACE WATERS.



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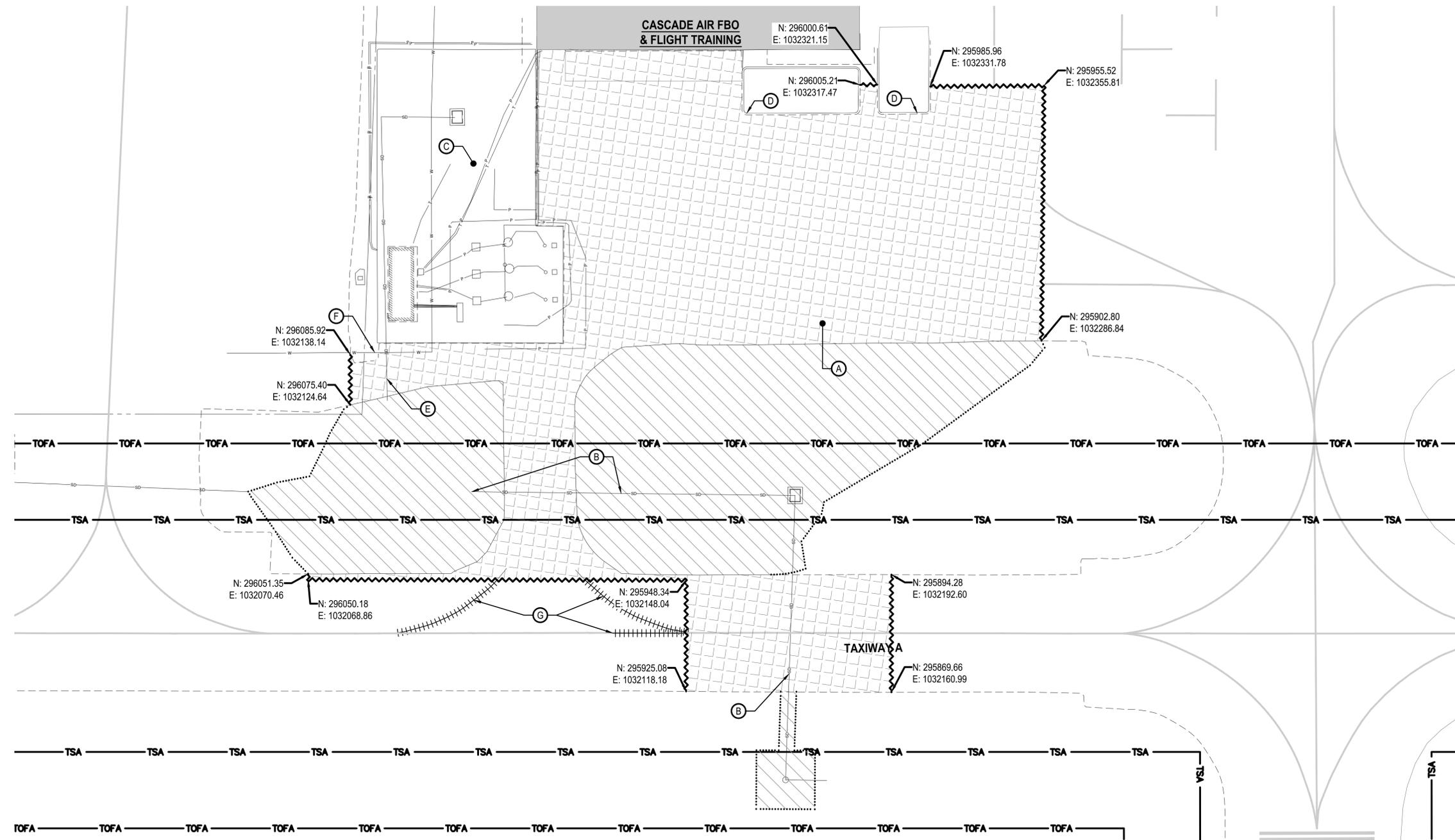
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**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION**

EROSION CONTROL NOTES AND DETAILS

DRAWING NO.
C-02

SHEET NO.
7 OF 15



DEMOLITION PLAN
SCALE: 1" = 20'

LEGEND

- PAVEMENT SAWCUT LINE, SEE GENERAL NOTE 3
- PAVEMENT MARKING REMOVAL
- PROPOSED GRADING LIMITS
- PROPOSED STRIPPING LIMITS, SEE GENERAL NOTE 4
- HMA REMOVAL

GENERAL NOTES

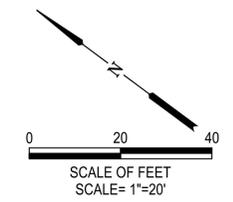
1. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. DAMAGES SHALL BE REPAIRED BY THE CONTRACTOR OR THE AFFECTED UTILITY AT THE CONTRACTOR'S EXPENSE.
2. UNLESS NOTED OTHERWISE, EXISTING INFRASTRUCTURE TO REMAIN SHALL BE PROTECTED.
3. SAWCUTTING OF HMA PAVEMENT SHALL BE INCIDENTAL TO PAVEMENT REMOVAL BID ITEM AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE.

GENERAL NOTES CONT.

4. EXISTING VEGETATED AREAS WITHIN THE GRADING LIMITS SHALL BE STRIPPED TO A DEPTH OF 4 INCHES.
5. FOR PAVEMENT MARKING REMOVALS, PROTECT TAXIWAY CENTERLINE DURING RADIUS REMOVAL.

KEYED NOTES

- (A) REMOVE AND DISPOSE OF EXISTING ASPHALT PAVEMENT.
- (B) REMOVE EXISTING STORM DRAIN.
- (C) PROTECT EXISTING CONCRETE PAD.
- (D) PROTECT EXISTING CURB.
- (E) PROTECT EXISTING STORM DRAIN.
- (F) PROTECT EXISTING WATERLINE.
- (G) PAVEMENT MARKING REMOVAL.



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NO.	DATE	BY	APPR	REVISIONS
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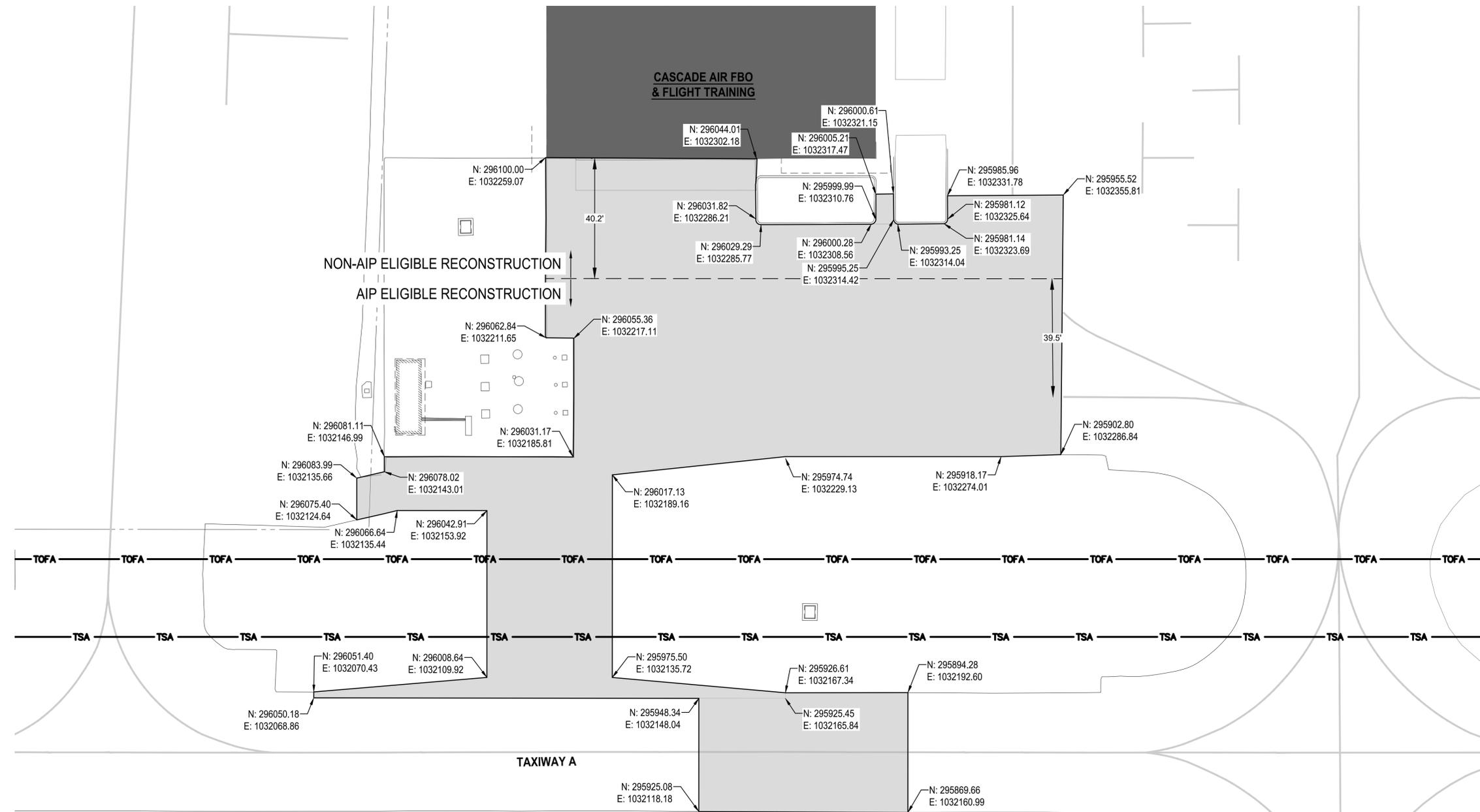
DATE: APRIL 2024
PROJECT NO: 35005.004.03

DESIGNED BY: TJS
DRAWN BY: TJS
CHECKED BY: CRL
SCALE: AS NOTED

**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXIWAY RECONSTRUCTION**

DEMOLITION PLAN

DRAWING NO. **C-03**
SHEET NO. **8 OF 15**



PAVING LAYOUT PLAN

SCALE: 1" = 20'

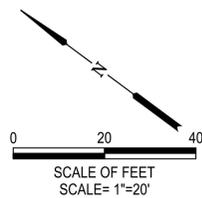


LEGEND

- EXISTING EDGE OF PAVEMENT
- PROPOSED EDGE OF PAVEMENT
- PROPOSED PAVEMENT

GENERAL NOTES

1. CONTRACTOR SHALL RECOMPACT EXISTING BASE MATERIAL TO FIRM AND UNYIELDING CONDITION.



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TJS
DRAWN BY:
TJS
CHECKED BY:
CRL
SCALE:
AS NOTED

DATE:
APRIL 2024

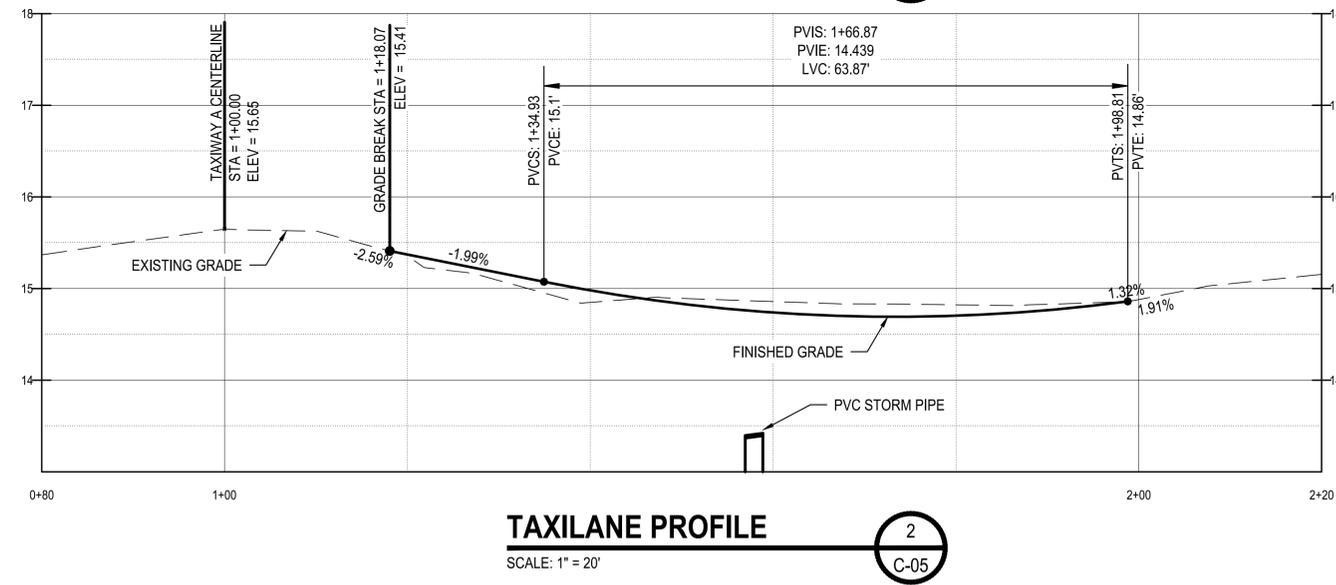
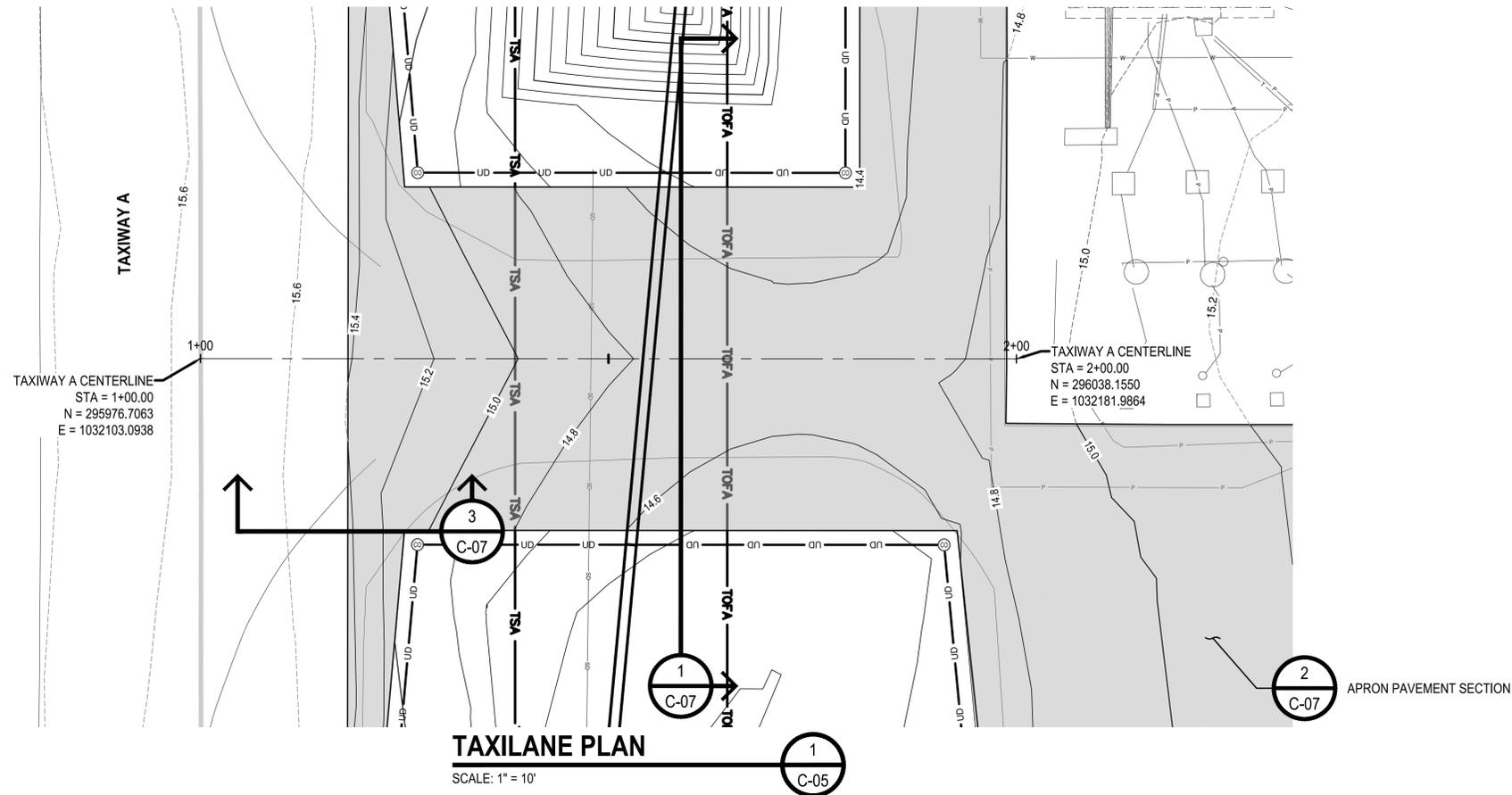
PROJECT NO:
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SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

PAVING LAYOUT PLAN

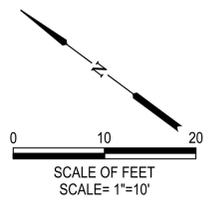
DRAWING NO.
C-04

SHEET NO.
9 OF 15



- LEGEND**
- EXISTING EDGE OF PAVEMENT
 - PROPOSED EDGE OF PAVEMENT
 - - 15.0 - - EXISTING 1' CONTOUR
 - - - - EXISTING 0.2' CONTOUR
 - 15.0 — PROPOSED 1' CONTOUR
 - - - - PROPOSED 0.2' CONTOUR
 - PROPOSED PAVEMENT
 - UD — PROPOSED UNDERDRAIN
 - PROPOSED CLEANOUT

- GENERAL NOTES**
1. FOR STORM IMPROVEMENTS, REFER TO RESPECTIVE DRAWINGS.
 2. SEED ALL AREAS DISTURBED BY CONTRACTOR OUTSIDE OF WORK AREA. AT NO ADDITIONAL COST TO THE OWNER.



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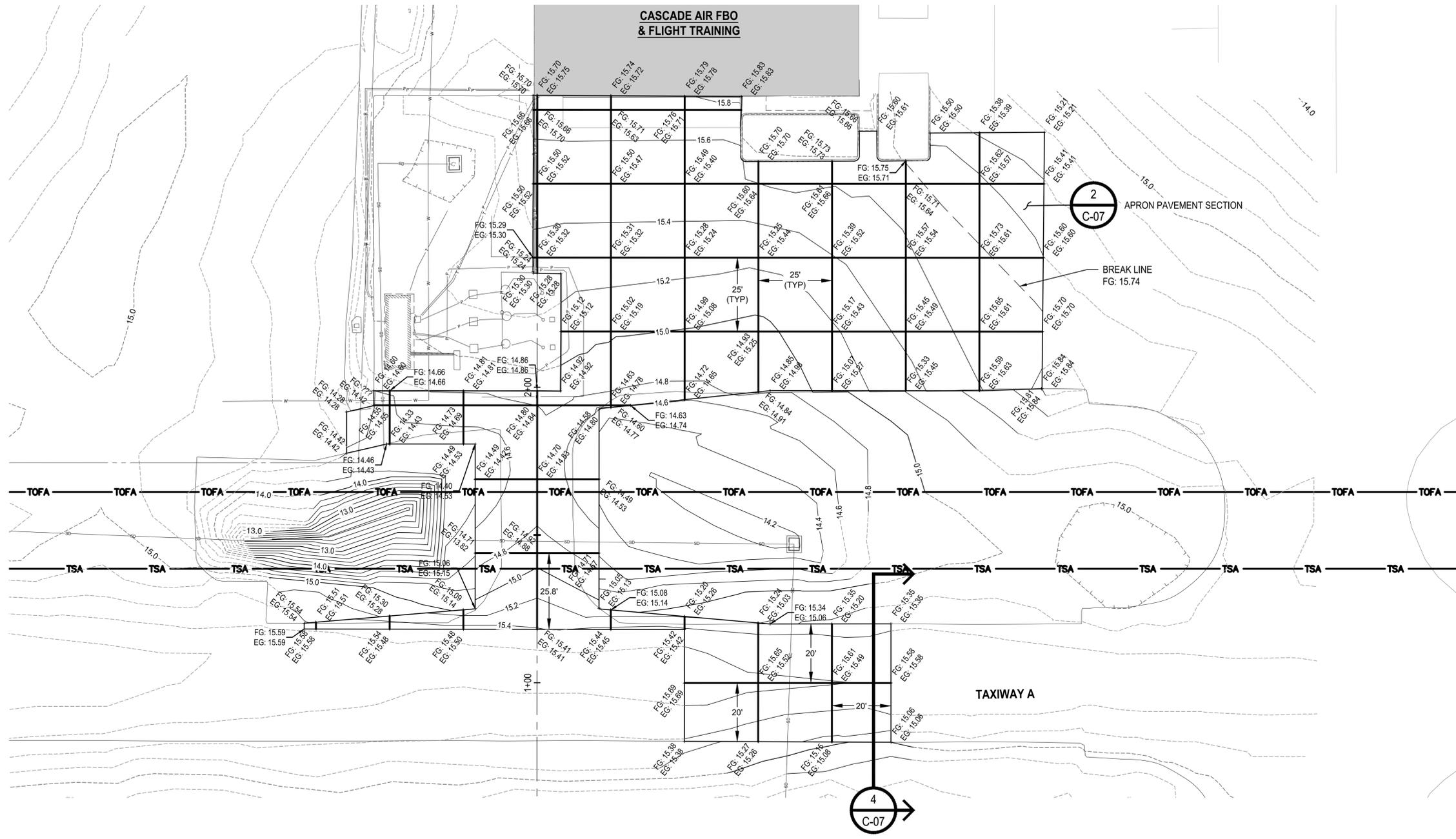
DATE: APRIL 2024
PROJECT NO: 35005.004.03

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CHECKED BY: CRL
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SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

TAXILANE PLAN AND PROFILE

DRAWING NO. **C-05**
SHEET NO. **10 OF 15**



PAVING PLAN AND POINT SCHEDULE

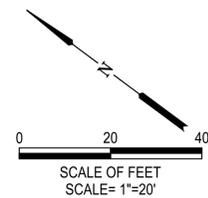
SCALE: 1" = 20'

LEGEND

- EXISTING EDGE OF PAVEMENT
- PROPOSED EDGE OF PAVEMENT
- - 15.0 - - EXISTING 1' CONTOUR
- - - - EXISTING 0.2' CONTOUR
- 15.0 — PROPOSED 1' CONTOUR
- — — PROPOSED 0.2' CONTOUR

GENERAL NOTES

1. FOR STORM IMPROVEMENTS, REFER TO RESPECTIVE DRAWINGS.
2. SEED ALL AREAS DISTURBED BY CONTRACTOR OUTSIDE OF WORK AREA. AT NO ADDITIONAL COST TO THE OWNER.



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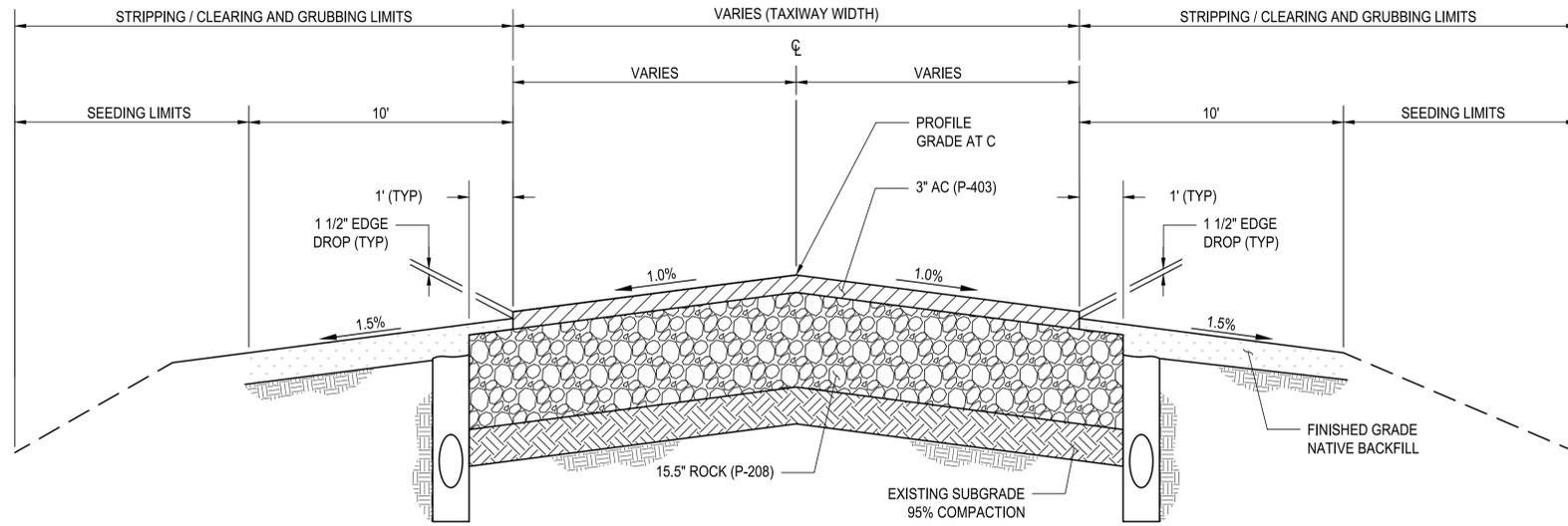
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**SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION**

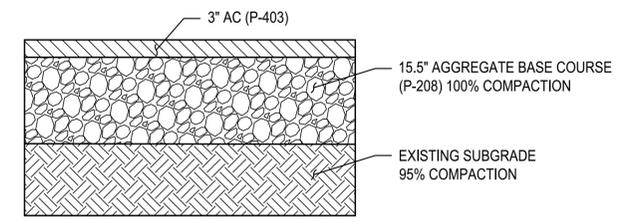
PAVING PLAN AND POINT SCHEDULE

DRAWING NO.
C-06

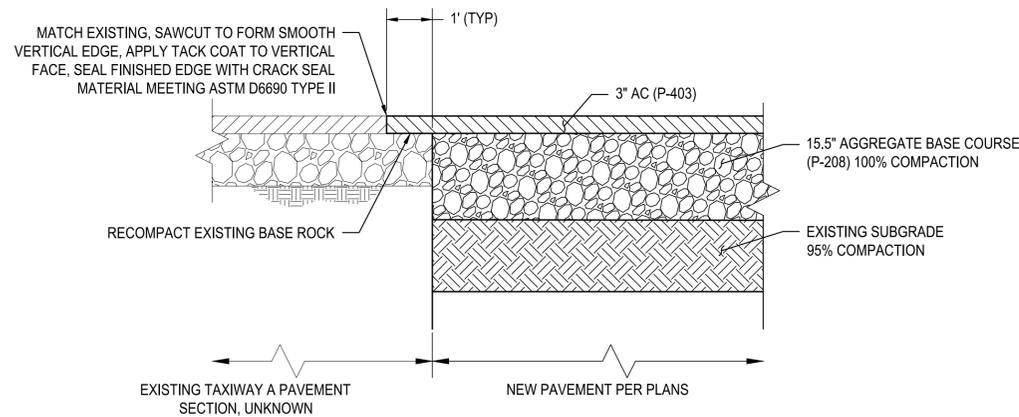
SHEET NO.
11 OF 15



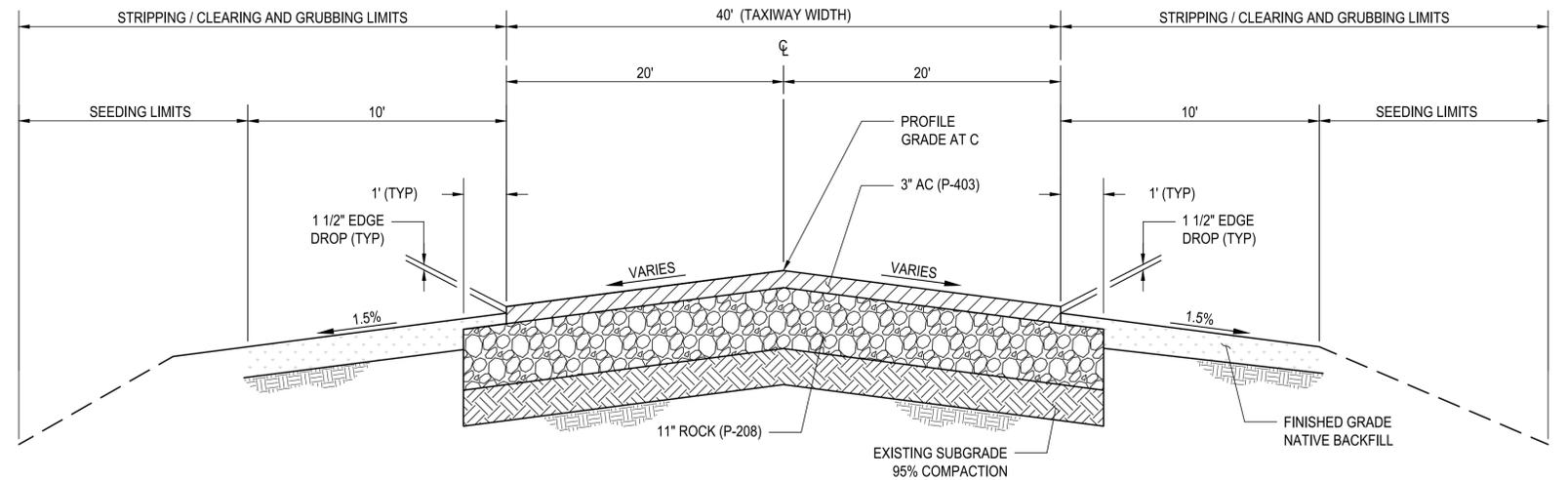
TYPICAL TAXILANE PAVEMENT SECTION 1
SCALE = NTS C-07



TYPICAL PAVMENT SECTION APRON 2
SCALE = NTS C-07



TYPICAL CONNECTION TO EXISTING PAVEMENT 3
SCALE = NTS C-07



TAXIWAY A SECTION 4
SCALE = NTS C-07

- NOTES:**
- CLEARING, GRUBBING LIMITS EXTEND FROM EXISTING EDGE OF PAVEMENT TO NEW CUT/FILL LIMITS.
 - ALL COMPACTION REQUIREMENTS PER ASTM D-698 UNLESS NOTED OTHERWISE.
 - EXCAVATION DEPTH TO REACH SUITABLE MATERIAL OR 18", WHICHEVER IS LESS. DEPTH AND LIMITS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

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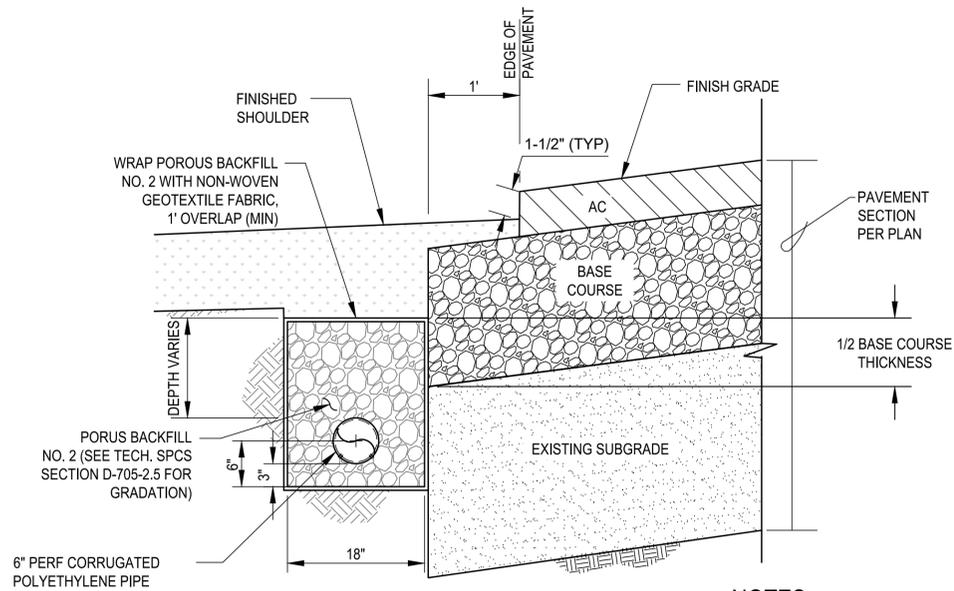
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CHECKED BY: CRL
SCALE: AS NOTED

DATE: APRIL 2024 PROJECT NO: 35005.004.03

SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

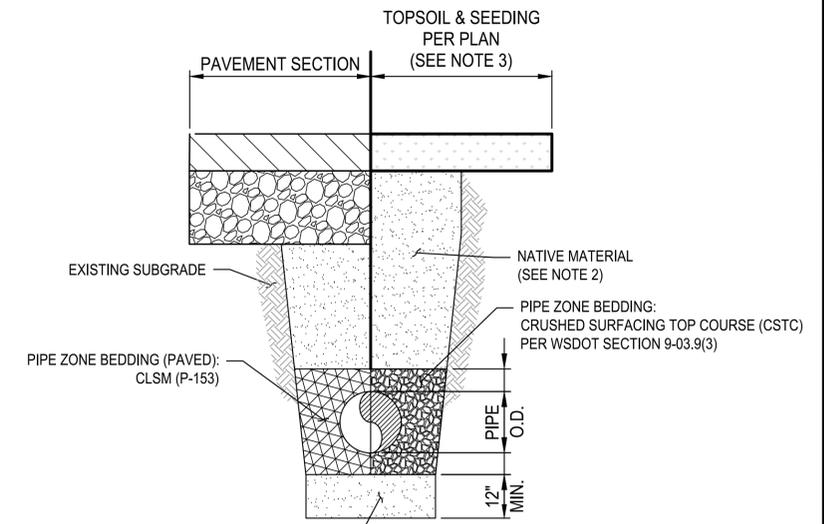
PAVEMENT SECTIONS

DRAWING NO. **C-07**
SHEET NO. **12 OF 15**



NOTES:

1. UNDERDRAIN BEDDING SHALL BE PLACED AND COMPACTED BEFORE INSTALLATION OF UNDERDRAIN PIPE PER ITEM D-705.



IF THE TRENCH BOTTOM IS DETERMINED TO BE UNSUITABLE BY THE ENGINEER, THE UNSUITABLE FOUNDATION MATERIAL SHALL BE REMOVED & REPLACED WITH PIPE FOUNDATION MATERIAL: AGGREGATE FOR GRAVEL BASE PER WSDOT SECTION 9-03.10

UNDERDRAIN DETAIL NON-PAVED AREAS

SCALE = NTS

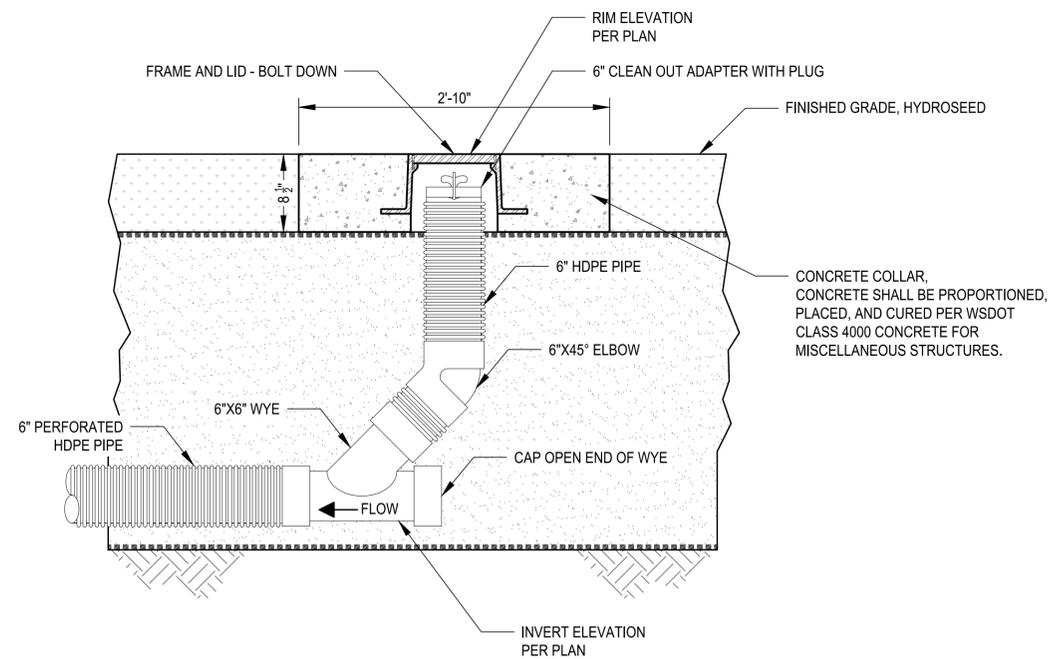


NOTES:

1. IN PAVED AREAS, TRENCH BACKFILL SHALL BE CLSM (P-153). IN NON-PAVED AREAS SHALL BE NATIVE BACKFILL THAT MEET REQUIREMENTS OF WSDOT SECTION 9-03.14(1). IF NOT, USE CRUSHED SURFACE TOP COURSE (CSTC) PER WSDOT SECTION 9-03.9(3).
2. ALL BACKFILL SHALL BE COMPACTED TO 95% MAXIMUM DENSITY.
3. TOPSOIL AND SEEDING SHALL BE MEASURED AND PAID FOR UNDER RESPECTIVE BID ITEMS AND NOT INCLUDED WITH THE 16" DUCTILE IRON STORM SEWER PIPE BID ITEM.

TRENCH BACKFILL DETAIL

SCALE = NTS



CLEANOUT DETAIL

SCALE = NTS



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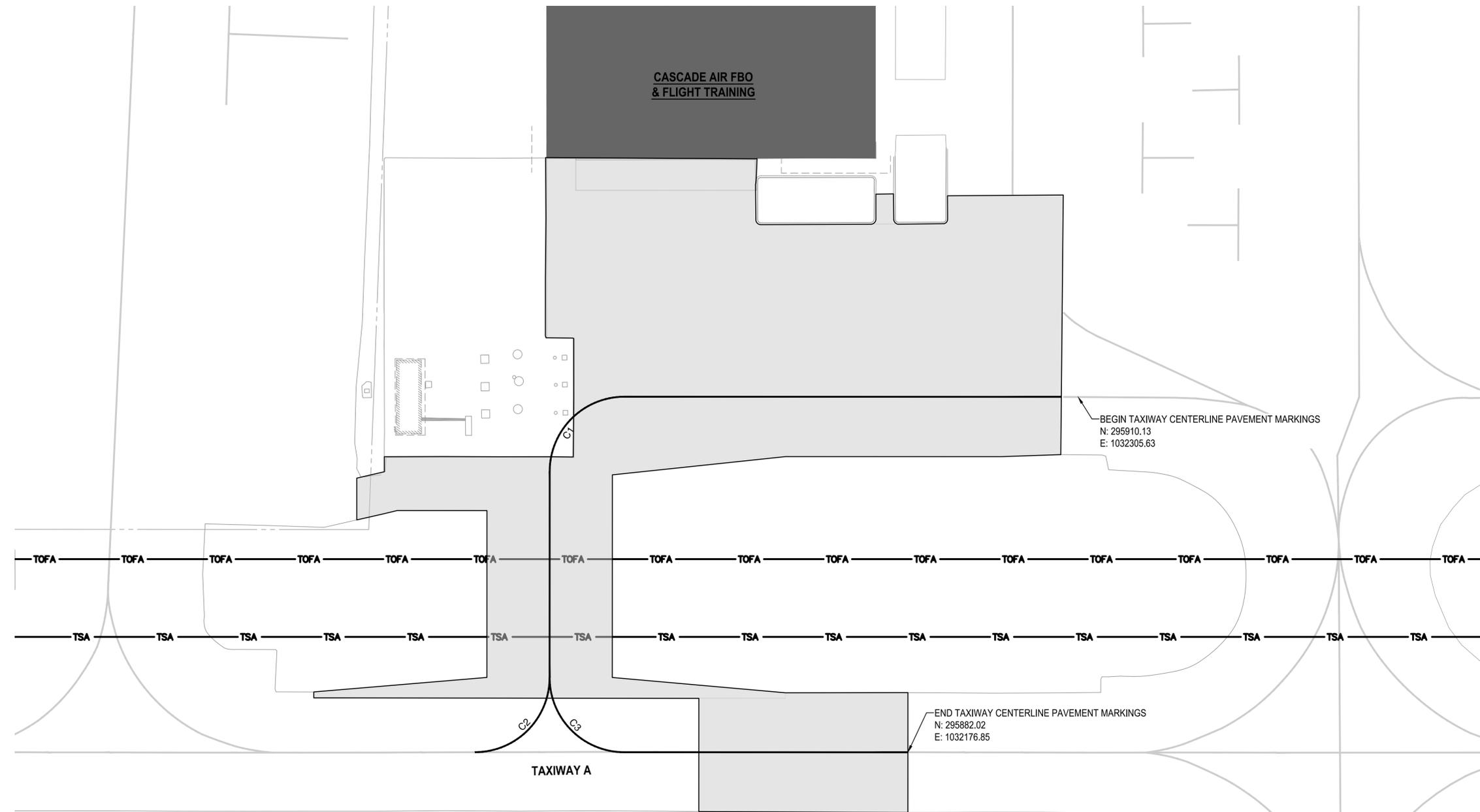
DATE: APRIL 2024
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SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

DRAINAGE DETAILS

DRAWING NO.
C-09

SHEET NO.
14 OF 15



PLAN
SCALE: 1" = 20'

LEGEND

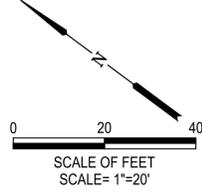
	EXISTING EDGE OF PAVEMENT
	EXISTING PAVEMENT MARKINGS
	PROPOSED EDGE OF PAVEMENT
	PROPOSED PAVEMENT MARKINGS
	PROPOSED PAVEMENT

- GENERAL NOTES**
- ALL TAXIWAY CENTERLINES YELLOW AND 6" WIDE, UNLESS OTHERWISE NOTED.
 - ANY PAINT SPILLING WILL BE CLEANED UP OR COVERED WITH BLACK PAINT AT CONTRACTORS EXPENSE.



TAXIWAY CENTERLINE STRIPE
NO SCALE

CURVE TABLE						
CURVE #	RADIUS	DELTA	START NORTHING	START EASTING	END NORTHING	END EASTING
C1	25.00	090.0000	296030.03	1032212.24	296034.39	1032177.151
C2	25.00	090.0000	295996.43	1032087.74	295992.07	1032122.820
C3	25.00	090.0002	295992.07	1032122.82	295956.99	1032118.460



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SOUTHWEST WASHINGTON REGIONAL AIRPORT
TASK ORDER #4 - FINAL DESIGN
APRON/TAXILANE RECONSTRUCTION

PAVEMENT MARKING PLAN AND DETAIL

DRAWING NO. **C-10**
SHEET NO. **15 OF 15**